

## STATEMENT OF ENVIRONMENTAL EFFECTS

### SKITUBE WALKWAY TO BRIDGE PERISHER VALLEY, PERISHER SKI RESORT KOSCIUSZKO NATIONAL PARK



**JANUARY 2016**

**Project: 71-15**

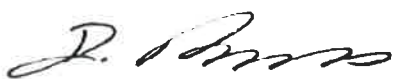
**Dabyne Planning Pty Ltd**

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## STATEMENT OF ENVIRONMENTAL EFFECTS

### SKITUBE WALKWAY TO BRIDGE PERISHER VALLEY, PERISHER SKI RESORT KOSCIUSZKO NATIONAL PARK

This report has been prepared by:



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Ivan Pasalich  
**Principal**  
Dabyne Planning Pty Ltd

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## 1. INTRODUCTION

Dabyne Planning Pty Ltd has been engaged by Perisher Blue Pty Ltd (Perisher), the operator of the Perisher Ski Resort, to prepare a Statement of Environmental Effects to accompany a Development Application (DA) to the NSW Department of Planning & Environment (DPE).

The application relates to the replacement of the existing concrete walkway crossing at the eastern end of the bridge which links the Perisher Centre and the Skitube terminal, in Perisher Valley.

The proposal is to replace the existing concrete path with a new heated walkway with appropriate water drainage to reduce the amount of slush and water that pools on the path and the all-weather gravel surface to either side.

A detailed description of the proposal is provided in Section 3 of the report.

The purpose of this SEE is to:

- describe the land to which the DA relates.
- describe the form of the proposed works.
- define the statutory planning framework within which the DA is to be assessed and determined; and
- assess the proposed development against the matters for consideration listed under Section 79C(1) of the Environmental Planning and Assessment Act, 1979 (EP&A Act, 1979).

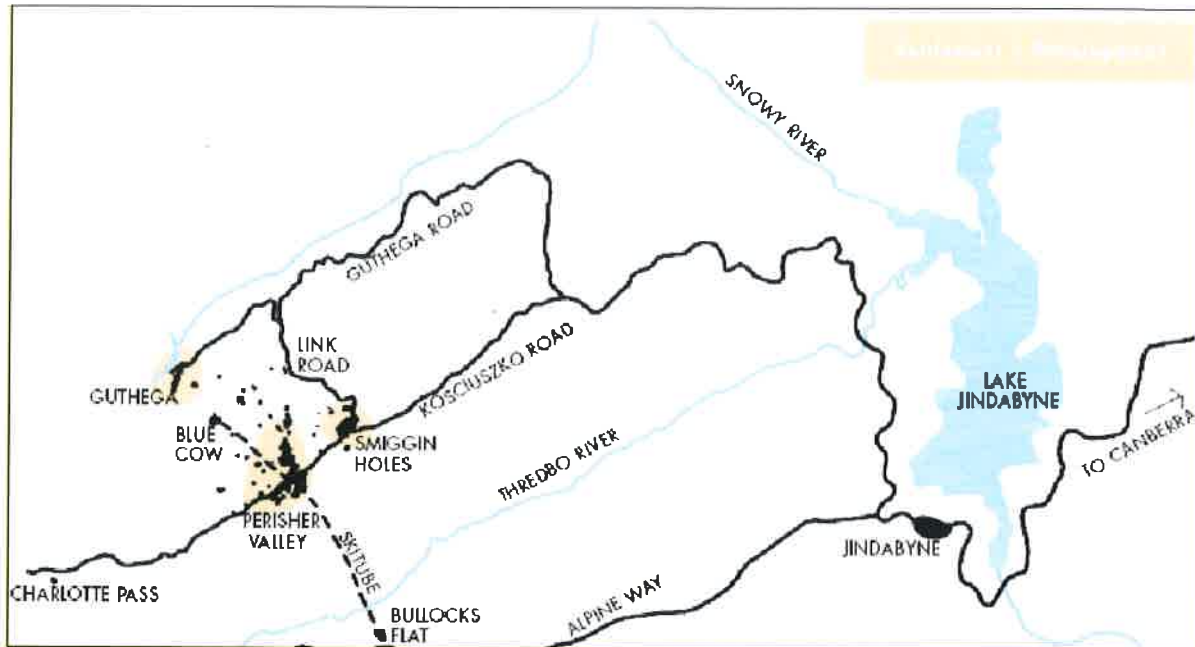
The report has been prepared in accordance with the requirements of Schedule 1 of the Environmental Planning and Assessment Regulations 2000.

## 2. THE LOCALITY & THE SITE

### 2.1 The Locality

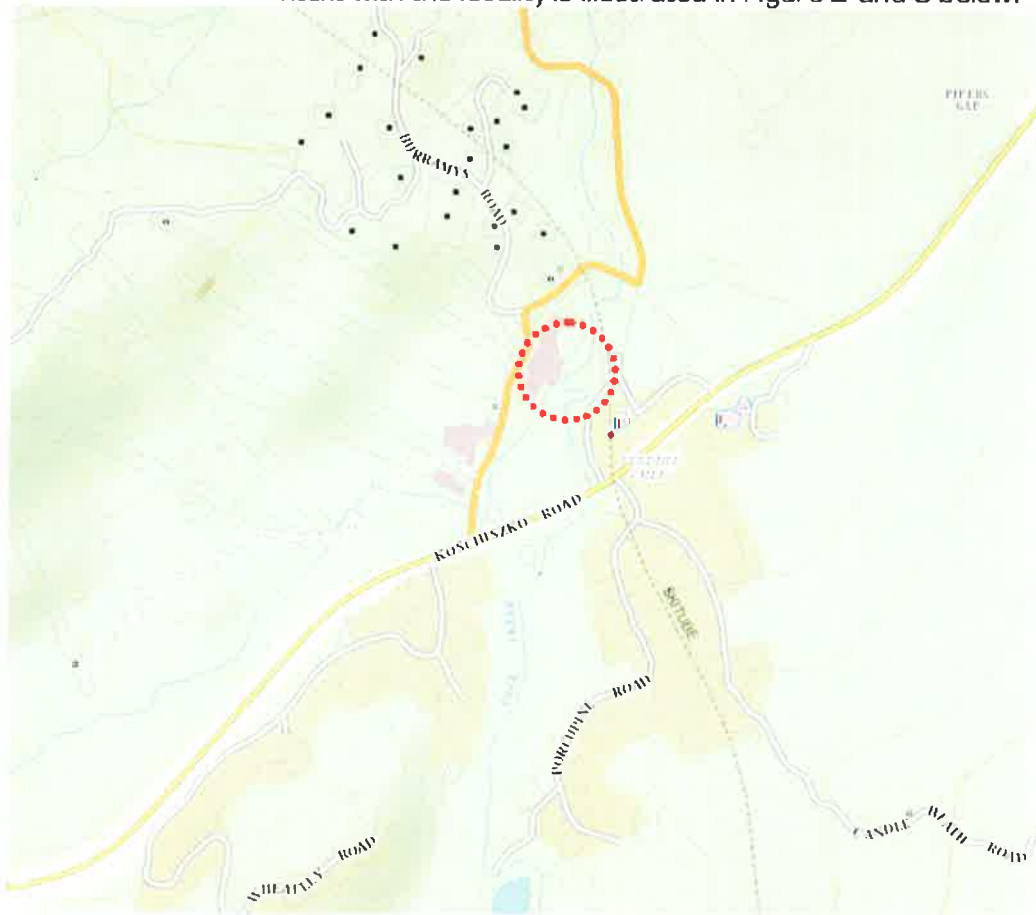
The subject site is located at the eastern end of the bridge linking the Perisher Centre with the Skitube terminal, at Perisher Valley, within the Perisher Range Resorts. Access to the site is achieved from the Kosciuszko Road.

The location of Perisher is illustrated in context with the regional locality below:

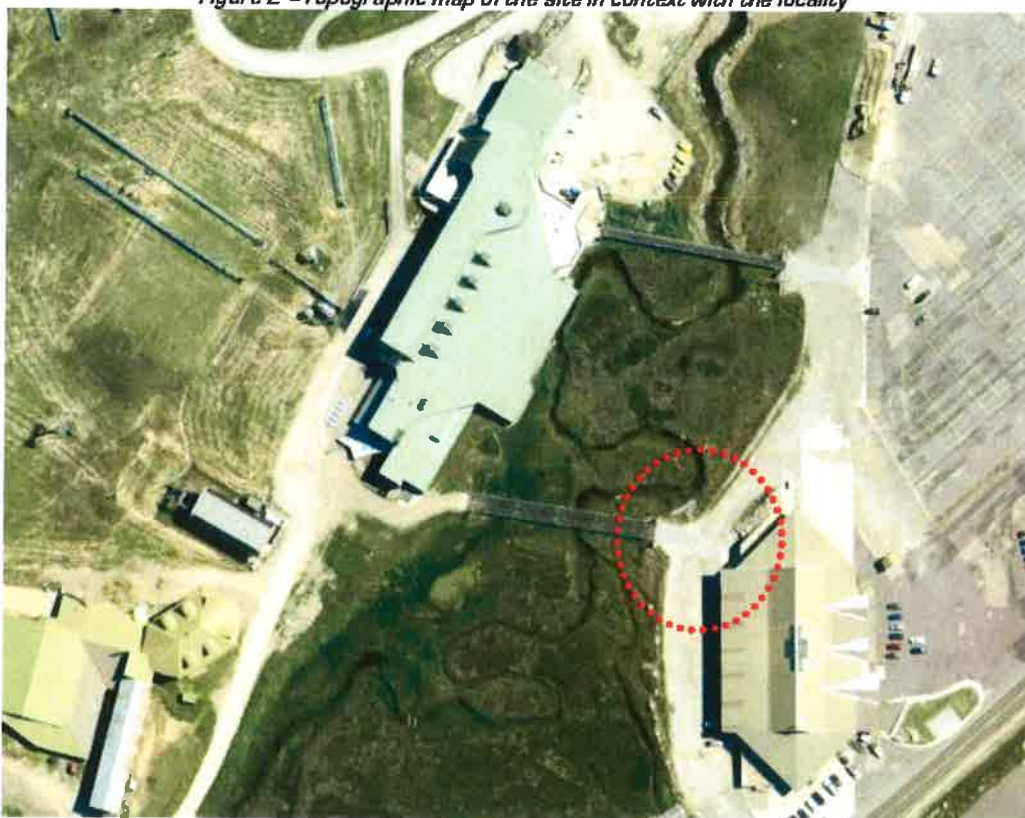


*Figure 1: Location of Blue Cow in context with the Region  
(source: Perisher Range Resorts Master Plan)*

The location of the site in context with the locality is illustrated in Figure 2 and 3 below:



*Figure 2 – Topographic map of the site in context with the locality*



*Figure 3: Aerial map of the site in context within the locality*

## 2.2 The Site

The subject site is located at the eastern end of the bridge connecting the Perisher Centre to the Skitube Terminal Building at Perisher Valley.

The location of the site in context with the Skitube Terminal building is provided in the aerial plan provided in figure 4 below:



*Figure 4: Location of the subject site in relation to the Skitube Terminal Building*

The path is currently a concrete path which intersects the all-weather gravel surface road which is the oversnow route during winter and an access road during summer.

The following photos identify the existing path and location of the proposed works:



*Figure 5: Photo of the existing path looking toward the Skitube Terminal*



*Figure 6: Photo across the existing path showing the entrance to the Skitube Terminal building*



*Figure 7: Photo of the existing pipe outlet and sediment basin below*



*Figure 8: Photo looking over the path from the Skitube Terminal towards the Perisher Centre and ski slopes*





*Figure 9: Photo of existing pipe outlet location in context with sediment basin and creek*

### **3. DESCRIPTION OF THE DEVELOPMENT**

#### **3.1 General Description**

The proposal is seeking consent for the approval to replace the concrete path linking the Skitube terminal to the bridge over Perisher Creek.

The path is currently a concrete path between two gravel sections of the oversnow transport route/access road. This oversnow route is one of the busiest in the resort during winter. The area is also a very high pedestrian traffic area as it links the Skitube Terminal and the car park to the Perisher Centre and ski slopes.

In winter, the path is frequently under snow. However due to the oversnow traffic and the pedestrian traffic, snow on the path gets very churned up and is frequently slushy. In very cold temperatures it becomes frozen and can be a pedestrian safety issue. In warmer temperatures it melts and the water forms deep puddles. The area becomes wet and muddy and is very unsightly and unpleasant to walk through.

The proposal is to replace the path with a new heated concrete path. The new path will be a hydronic heated convex 180mm thick concrete slab. Electricity will be used to heat the slab. The path will be constructed using 40Mpa concrete. The new walkway will be approximately 5m wide with a maximum 3% side crossfall.

Introducing a heating element to the path will assist with managing snow, ice and water on the path. To deal with stormwater, two new 1000mm x 1000mm grated surface entry stormwater pits will be constructed, replacing the single stormwater pit, with hydronic heating to the concrete base of the pit. The existing 300mm stormwater pipe will be replaced with a 450mm diameter concrete pipe, with the existing rock outlet replaced and upgraded.

The existing paved area between the Skitube terminal and path will be retained.

## 4. ENVIRONMENTAL PLANNING ASSESSMENT

### 4.1 SECTION 79C(1)(a)(i) – ENVIRONMENTAL PLANNING INSTRUMENTS

The only applicable Environmental Planning Instrument to the proposed development and site is State Environmental Planning Policy (Kosciuszko National Park – Alpine Resorts) 2007 (SEPP Alpine Resorts). The relevant clauses contained within SEPP Alpine Resorts are addressed below:

#### Land Use Table:

The Land Use Table for Perisher Range Resorts specifies that “infrastructure facilities” are permitted with consent. Infrastructure facilities is defined in the SEPP as: *“facilities provided as utility undertakings (whether or not as public utility undertakings), including any of the following:*

- (a) water storage dams, including artificial reservoirs, tanks and ponds,*
- (b) roads (other than roads provided by or on behalf of the Roads and Traffic Authority),*
- (c) sewage treatment works,*
- (d) waste management facilities”.*

The path forms part of the access road so is permissible under clause (b).

“Recreation infrastructure” is also permitted with consent. In the SEPP Alpine Resorts “recreation infrastructure” is defined as infrastructure provided for the purposes of active or passive recreation for tourists, including walking trails, mountain bike trails, directional signage, cross country ski trails and oversnow routes.

The proposed works are therefore permissible with consent under either land use definition.

#### Clause 14 - Matters for consideration:

Matter for Consideration	Response
Cl.14 (1) In determining a development application that relates to land to which this Policy applies, the consent authority must take into consideration any of the following matters that are of relevance to the proposed development:	
(a) the aim and objectives of this Policy, as set out in clause 2,	<i>The proposed alterations are considered to result in a development that is consistent with the aims and objectives set out in clause 2 of the SEPP.</i>
(b) the extent to which the development will achieve an appropriate balance between the conservation of the natural environment and any measures to mitigate environmental hazards (including geotechnical hazards, bush fires and flooding).	<i>The proposed alterations do not require any mitigation measures for environmental hazards.</i>

<p>c) having regard to the nature and scale of the development proposed, the impacts of the development (including the cumulative impacts of development) on the following:</p> <p>(i) the capacity of existing transport to cater for peak days and the suitability of access to the alpine resorts to accommodate the development,</p> <p>(ii) the capacity of the reticulated effluent management system of the land to which this Policy applies to cater for peak loads generated by the development,</p> <p>(iii) the capacity of existing waste disposal facilities or transfer facilities to cater for peak loads generated by the development,</p> <p>(iv) the capacity of any existing water supply to cater for peak loads generated by the development,</p>	<p><i>The proposed alterations will not affect the capacity of the existing transport, reticulated effluent management system, waste disposal facilities or existing water supply within the resort.</i></p>
<p>(d) any statement of environmental effects required to accompany the development application for the development,</p>	<p><i>This Statement of Environmental Effects satisfies this sub-clause.</i></p>
<p>(e) if the consent authority is of the opinion that the development would significantly alter the character of the alpine resort—an analysis of the existing character of the site and immediate surroundings to assist in understanding how the development will relate to the alpine resort,</p>	<p><i>The proposed alterations will not alter the character of the resort.</i></p>
<p>(f) the Geotechnical Policy—Kosciuszko Alpine Resorts (2003, Department of Infrastructure, Planning and Natural Resources) and any measures proposed to address any geotechnical issues arising in relation to the development</p>	<p><i>The proposed works are replacing an existing path located outside of the 'G' line, the area associated with Geotechnical Risk.</i></p> <p><i>As the works do not require any footings, a Form 4A is also not required.</i></p> <p><i>Notwithstanding this, the Applicant has chosen to engage JK Geotechnics to prepare a Form 4, which will be submitted with the DA separately.</i></p>
<p>(g) if earthworks or excavation works are proposed—any sedimentation and erosion control measures proposed to mitigate any adverse impacts associated with those works,</p>	<p><i>Excavation works are required for the removal of the existing slab and pipes and replacement of slab and pipes. Sediment and erosion controls as outlined in the SEMP provided in Appendix A will be required.</i></p>

<p>(h) if stormwater drainage works are proposed—any measures proposed to mitigate any adverse impacts associated with those works,</p>	<p><i>The proposal seeks to upgrade and improve stormwater drainage works associated with the pathway. These improvements include melting snow and ice on the walkway and increasing the drainage size and pipe size. This will result in less water pooling and a more efficient management of stormwater drainage.</i></p>
<p>(i) any visual impact of the proposed development, particularly when viewed from the Main Range,</p>	<p><i>The proposed alterations will have no visual impact.</i></p>
<p>(j) the extent to which the development may be connected with a significant increase in activities, outside of the ski season, in the alpine resort in which the development is proposed to be carried out,</p>	<p><i>The proposed alterations will not be expected to increase any activities outside of the ski season.</i></p>
<p>(k) if the development involves the installation of ski lifting facilities and a development control plan does not apply to the alpine resort:</p> <p>(i) the capacity of existing infrastructure facilities, and</p> <p>(ii) any adverse impact of the development on access to, from or in the alpine resort,</p>	<p><i>Not applicable.</i></p>
<p>(l) if the development is proposed to be carried out in Perisher Range Alpine Resort:</p> <p>(i) the document entitled Perisher Range Resorts Master Plan, as current at the commencement of this Policy, that is deposited in the head office of the Department, and</p> <p>(ii) the document entitled Perisher Blue Ski Resort Ski Slope Master Plan, as current at the commencement of this Policy, that is deposited in the head office of the Department,</p>	<p><i>The Perisher Range Resorts Master Plan proposes a raised pedestrian bridge at this location, which would be constructed above the new pathway. Oversnow traffic will continue to use the existing path under the new raised bridge, therefore the new pathway is considered to be consistent with the Perisher Range Resorts Master Plan.</i></p>
<p>(m) if the development is proposed to be carried out on land in a riparian corridor:</p> <p>(i) the long term management goals for riparian land, and</p> <p>(ii) whether measures should be adopted in the carrying out of the development to assist in meeting those goals.</p>	<p><i>The proposed works will be undertaken within 40m of Perisher Creek, a riparian corridor.</i></p> <p><i>An assessment of the long term management goals for riparian land, is provided below.</i></p>
<p>(2) The long term management goals for riparian land are as follows:</p>	

<p>(a) to maximise the protection of terrestrial and aquatic habitats of native flora and native fauna and ensure the provision of linkages, where possible, between such habitats on that land.</p>	<p><i>The proposed replacement of a concrete pathway, drains and pipes within a highly disturbed environment will have no additional impacts on terrestrial and aquatic habitats of native flora and native fauna, given the limitations of the work, their scale and location within exotic vegetation.</i></p>
<p>(b) to ensure that the integrity of areas of conservation value and terrestrial and aquatic habitats of native flora and native fauna is maintained,</p>	<p><i>The integrity of this area of Perisher Creek, with its eastern embankment being highly modified and disturbed, will be maintained by these works.</i></p>
<p>(c) to minimise soil erosion and enhance the stability of the banks of watercourses where the banks have been degraded, the watercourses have been channelised, pipes have been laid and the like has occurred.</p>	<p><i>Soil erosion will be managed during construction as outlined in the SEMP and through an upgraded outlet pipe as part of the construction.</i></p>
<p>(3) A reference in this clause to land in a riparian corridor is a reference to land identified as being in such a corridor on a map referred to in clause 5.</p>	

#### **4.2 SECTION 79C(1)(a)(ii) – DRAFT ENVIRONMENTAL PLANNING INSTRUMENTS**

There are no draft Environmental Planning Instruments that are applicable to the site or proposed development.

#### **4.3 SECTION 79C(1)(a)(iii) – DEVELOPMENT CONTROL PLANS**

There are no Development Control Plans applicable to the Kosciuszko Alpine Resorts under State Environmental Planning Policy (Kosciuszko National Park – Alpine Resorts) 2007.

#### **4.4 SECTION 79C(1)(a)(iiia) – PLANNING AGREEMENTS**

There are no Planning Agreements applicable to the Kosciuszko Alpine Resorts under State Environmental Planning Policy (Kosciuszko National Park – Alpine Resorts) 2007.

#### **4.5 SECTION 79C(1)(a)(iv) – REGULATIONS**

The development application has been made in accordance with the requirements contained in Clause 50(1A) of the Environmental Planning and Assessment Regulation 2000.

#### **4.6 SECTION 79C(1)(b) – LIKELY IMPACTS**

##### **Natural Environment:**

The proposed development will not increase the quantity of water dispersed into Perisher Creek because the catchment area remains the same, however a larger pipe and stormwater pits will result in the water being dispersed more expediently reducing surface water pooling.

The outlet dispersing water into the sediment pond will be upgraded, but remain in the same location.

The entire work site is currently a concrete path, gravel driveway or on the edge where the stormwater pipe outlet is located, exotic grasses with no native vegetation to be disturbed, as shown in figure 9 above. This area is not classified as part of an Endangered Ecological Community.

It is considered that the development will have a positive effect on the natural environment with a marginal improvement in the quality of the water being dispersed into Perisher Creek due to the reduced water retention on the bridge and fewer oversnow vehicles and pedestrians walking through the pooled water.

**Built Environment:**

The proposed alterations are considered to have no effect on the built environment.

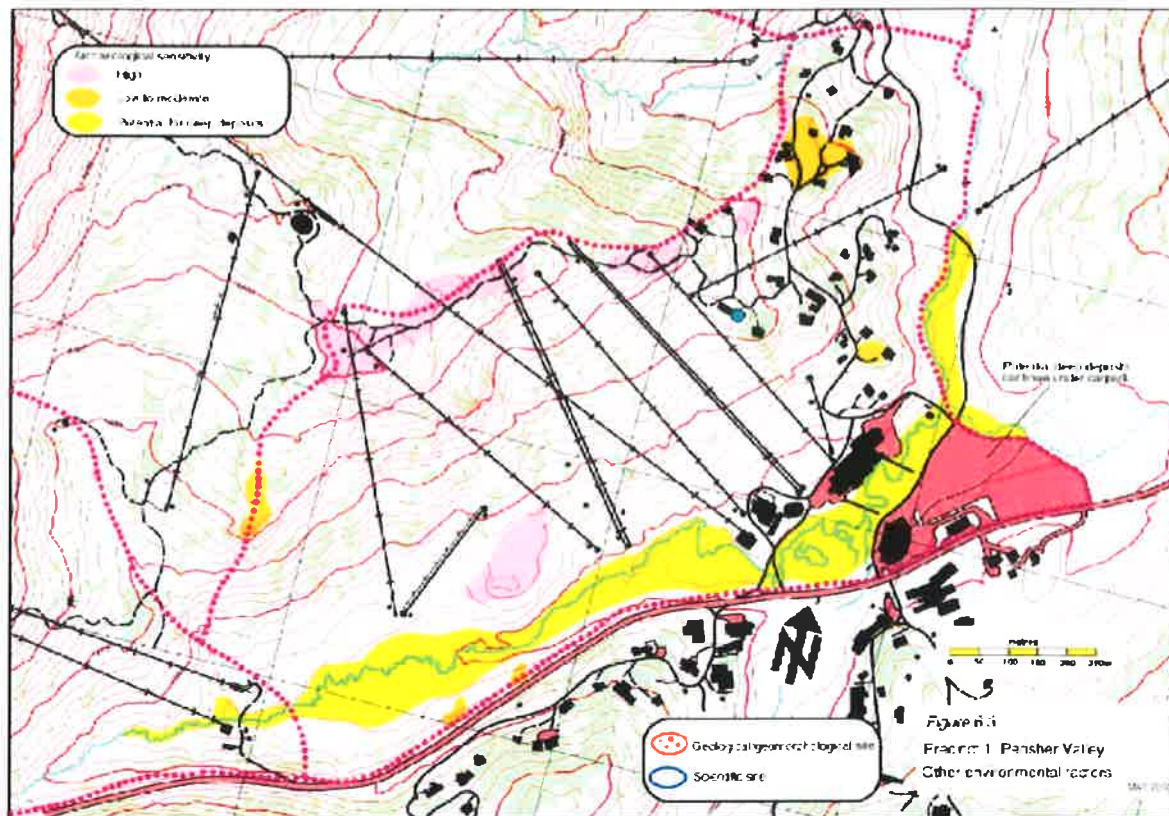
**Social and Economic impacts in the locality:**

The social impacts generated by the approval for the development are considered to be positive with an improved experience for guests entering and exiting the resort from the carpark and Skitube terminal. Safety will be improved by the elimination of slush and ice on the path, and there will be an improved visual impact and guest experience provided by the elimination of puddles of water on the path.

*Aboriginal Heritage:*

With respect to Aboriginal Heritage, the proposed works are being undertaken within the footprint of the existing pathway and gravel road and will only result in the existing drains and pipes being replaced and upgraded, which is a highly disturbed and modified site. In accordance with the predictive model that mapped the zones of archeological sensitivity undertaken by Navin Officer Heritage Consultants as part of the Perisher Range Resorts Environmental Study, undertaken in 2000 by Connell Wagner, the area is not mapped as part of the predicted 'high sensitivity' area. The area is mapped for potential for deep deposits, however the removal of a concrete slab and drains would not impact on this, given the estimated 1m of fill in this area.

An extract of this map from the Perisher Valley Precinct in the Perisher Ski Slope Master Plan which is based on the predictive model undertaken by Navin Officer for Connell Wagner, is provided below in figure 10.



**Figure 10: Other environmental factors maps for the Perisher Valley Precinct**  
 (source: PSSMP)

The economic impacts generated by the approval for the external alterations are expected to be minimal but positive due to the additional short term construction jobs created.

#### **4.7 SECTION 79C(1)(c) – SUITABILITY OF THE SITE**

The proposed development is replacing an existing pathway so the subject site is considered suitable to accommodate the proposed development.

#### **4.8 SECTION 79C(1)(d) – SUBMISSIONS**

It is not considered necessary for the consent authority to undertake any notification process.

#### **4.9 SECTION 79C(1)(e) – THE PUBLIC INTEREST**

The above assessment has demonstrated that the proposal satisfies the objectives and relevant clauses prescribed under State Environmental Planning Policy (Kosciuszko National Park – Alpine Resorts) 2007.

The development is considered to be within the public interest, as it will result in improved public safety and an improved guest experience as they enter and exit the resort.



## **5. CONCLUSION**

The proposed development has been considered in regard Section 79C of the EP&A Act, 1979 and State Environmental Planning Policy (Kosciuszko National Park - Alpine Resorts) 2007.

The proposed development has been found to be consistent with the above legislation and Environmental Planning Instrument, as detailed in the above report.

The proposed works will improve the visitor amenity and will result in improved public safety and an improved guest experience as they enter and exit the resort. The proposal is therefore considered to be within the public interest and on balance is considered an appropriate form of development for the site.

# **APPENDIX A**

## **SITE ENVIRONMENTAL MANAGEMENT PLAN**

# APPENDIX A SITE ENVIRONMENTAL MANAGEMENT PLAN Skitube Walkway Crossing to Bridge, Perisher Valley

## 1. Introduction

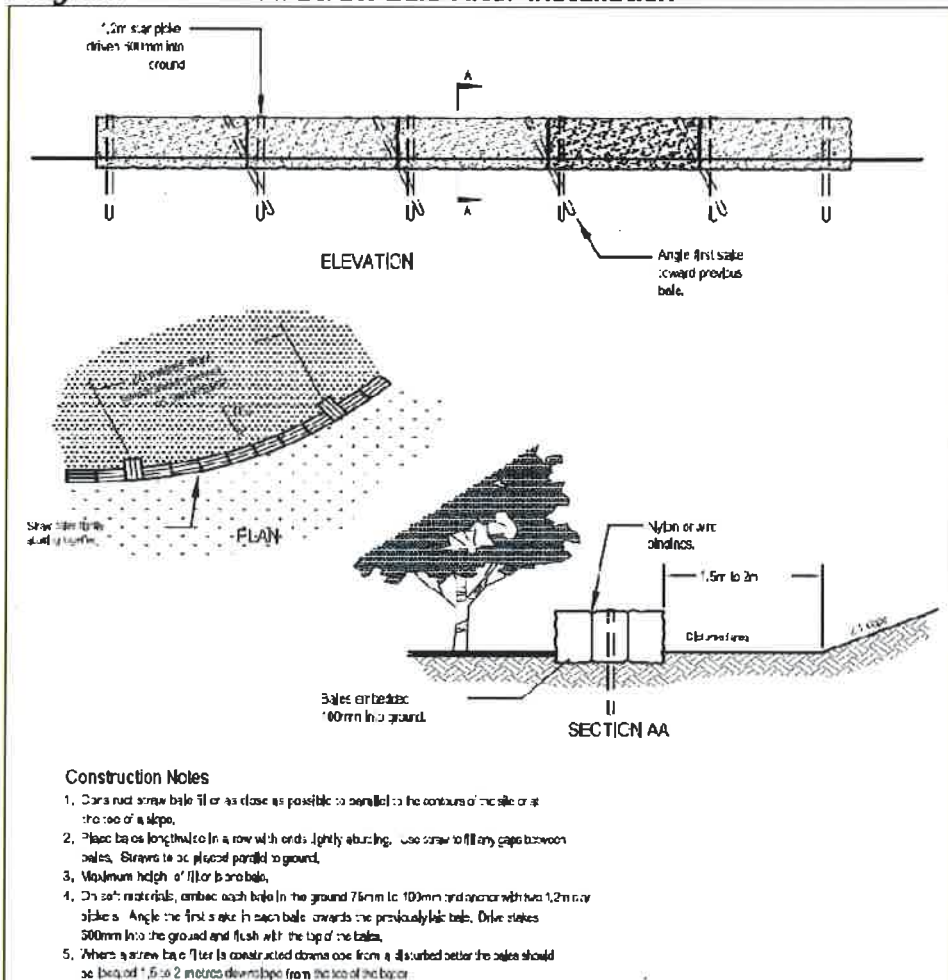
The proposed works require excavation works to remove the existing slab, drainage pits and pipe.

The following plan has been provided to identify the appropriate location for erosion and sediment controls, construction vehicle parking and material storage to assist in minimising any impacts arising from the construction works.

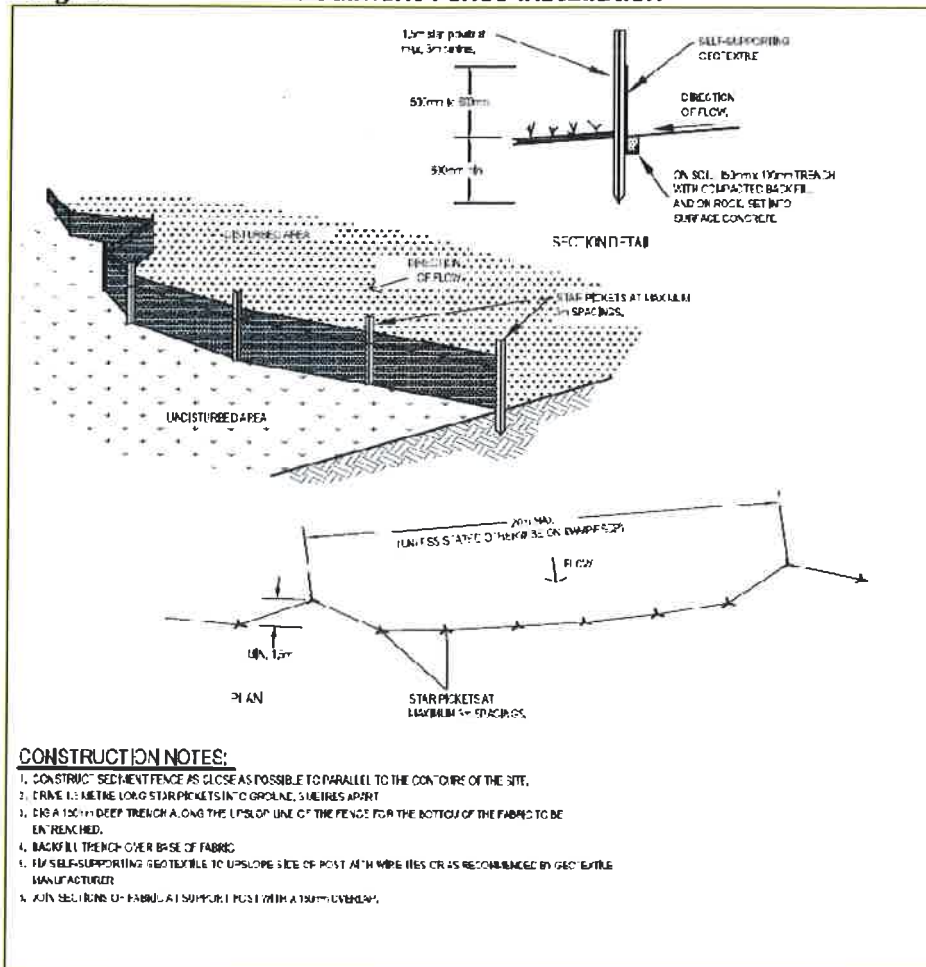
## 2. Erosion and Sedimentation Control

Appropriate environmental management controls may be required to manage soil and surface water during the construction of the development. Temporary controls will include either a straw bale filter, installed as illustrated Diagram A or a sediment fence in accordance with Diagram B below.

**Diagram A: Standard Straw Bale Filter Installation**



**Diagram B: Standard Sediment Fence Installation**



The controls are to be installed in accordance with the following suite of criteria:

- Both straw bale and sediment control fencing should be installed on the low side of the work site (between the works and Perisher Creek); and
- Both straw bale and sediment control fencing should be installed as close as possible to follow the existing contours of the site.

All excavated material is to be placed on the high side of the sediment control within disturbed areas only.

The existing drain and pipe is to be protected from sediment loss, when the concrete slab is removed.

### 3. Access & Vehicle Parking

Access to the site will be achieved via the existing Access Road with parking for construction vehicles available in the Perisher carpark as identified in figure's 1 & 2 below.

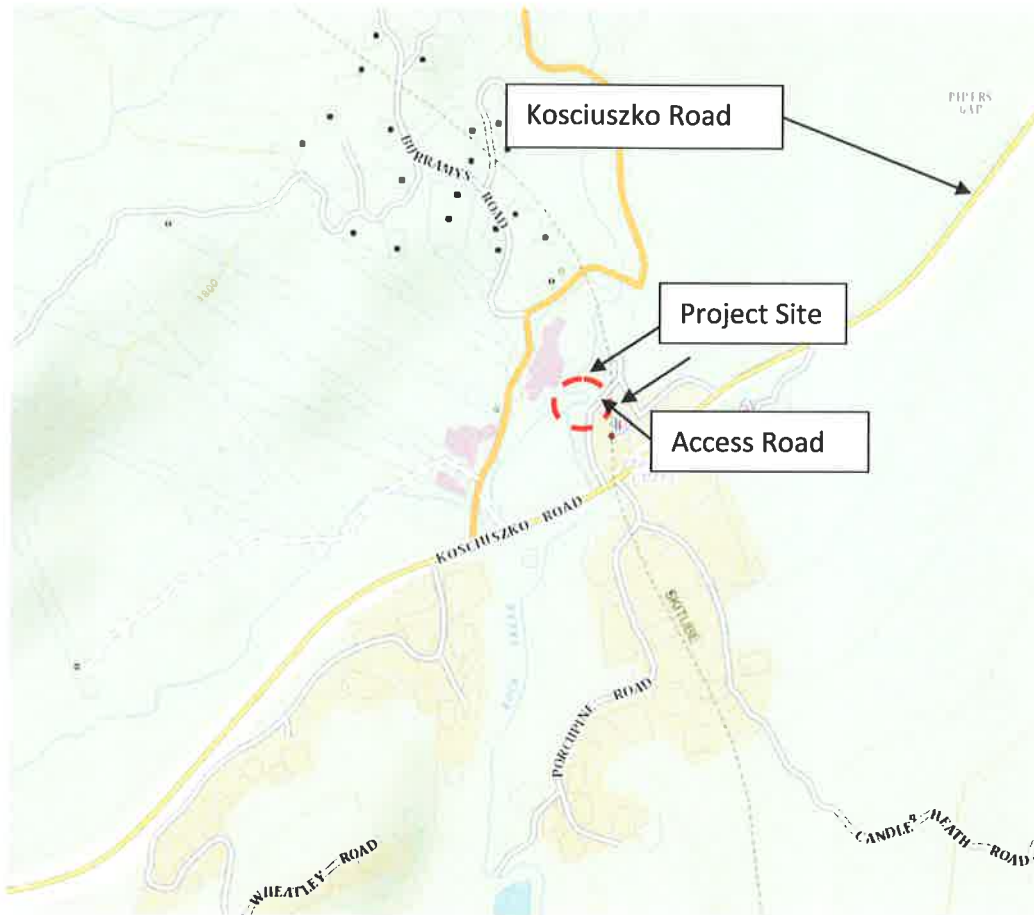
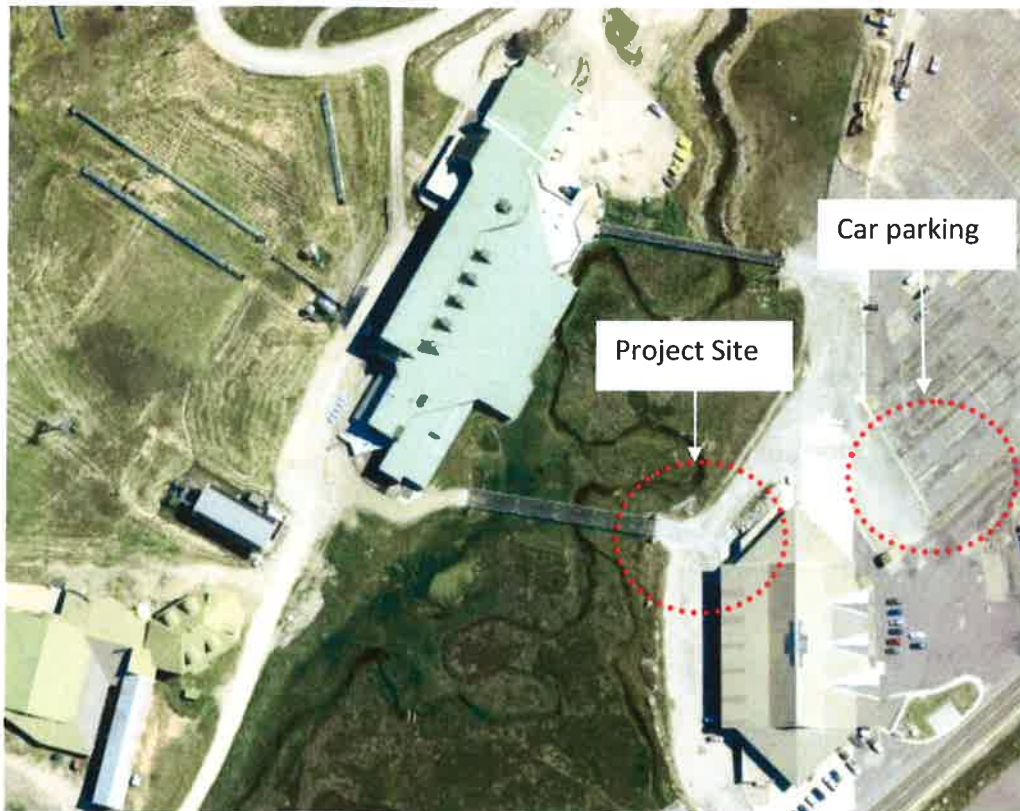


Figure 1 - Topographic map showing Access Road



*Figure 2 – Aerial map showing access road and car parking area.*

A 30 tonne excavator with hydraulic hammer will be used to remove the existing slab, drain and pipes. Direct access to the site for a dump truck plus concrete truck can be achieved.

This work can all be located on disturbed land.

#### **4. Material Storage**

Materials can be placed temporarily along the side of the access road as shown in figure 3 below.

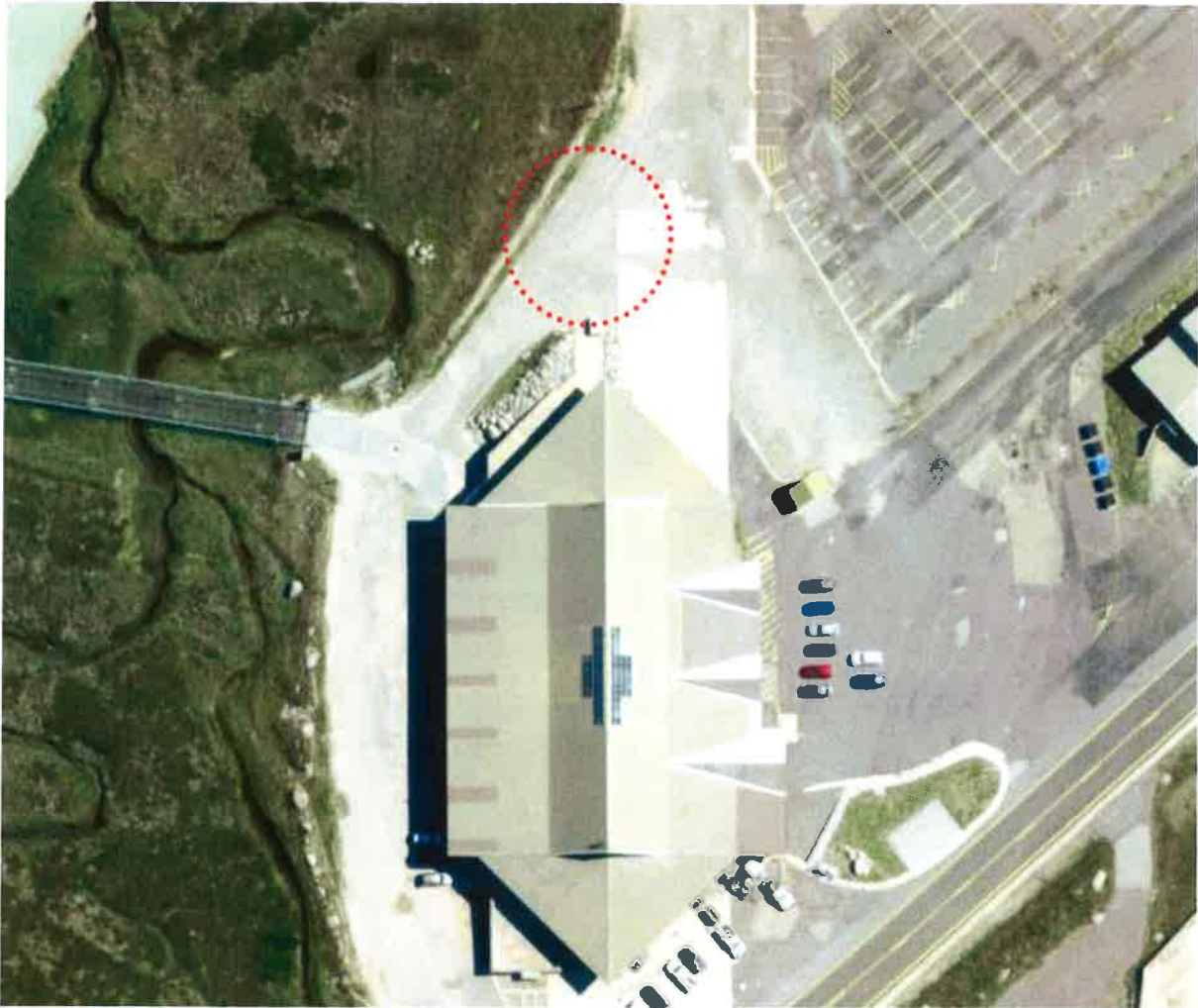


Figure 3 – material storage area outlined in red.

## 5. Waste Management

To ensure that waste is managed, the following controls and measures are to be adhered to:

- All litter generated on site is to be disposed of in appropriate bin provided on site and disposed at Jindabyne tip.
- All employees shall be informed of the need to maintain a clean worksite.
- Site generated waste including garbage, concrete and excess materials shall be collected within the waste bin and removed from the site to landfill located in Jindabyne.
- All loads of rubbish removed shall be securely covered to ensure no spillage.
- To the furthest extent possible efforts shall be made to reduce, reuse and recycle materials used onsite.
- The worksite shall be left in a tidy and rubbish free state upon completion of the Project.

## 6. Noise and vibration pollution

The intended hours of operation is from 7am to 5pm Monday to Friday, 8am – 5pm on Saturday with no work on Sundays or Public Holidays. No construction is to take place from June through to September.

Noise pollution is not expected to be an issue from the construction of the proposed works.

## 7. Air pollution

The construction of the proposed development is not expected to create any unnecessary air pollution.

## 8. Fuels and Chemicals

The proposed development will not require the storage of fuels or chemicals on site for construction. The supplementary tank will be filled after installation.

## 9. Emergency Procedures

In case of an emergency, the following key emergency response contacts are provided below:

### Key Emergency Response Contacts

Organisation	Emergency Phone	Non Emergency Phone
NSW Police	000	Jindabyne: 6456 2244
NSW Fire and Rescue	000	Perisher: 6457 5037 Jindabyne: 6456 2476
NSW Ambulance	000	Perisher: 131 233
Medical Centres	Perisher (Winter Only): 6457 5266 Jindabyne: 6457 1221	
National Parks and Wildlife Service (NPWS)/OEH	1800 629 104	Perisher: 6457 4444 Jindabyne 6450 5555
Roads and Maritime Services	Traffic incidents & road conditions: 131 700 Road closures and special events: 132 701	
Environment Protection Authority Environment Line	131 555	
NRMA Road Service	Jindabyne: 6456 1159	